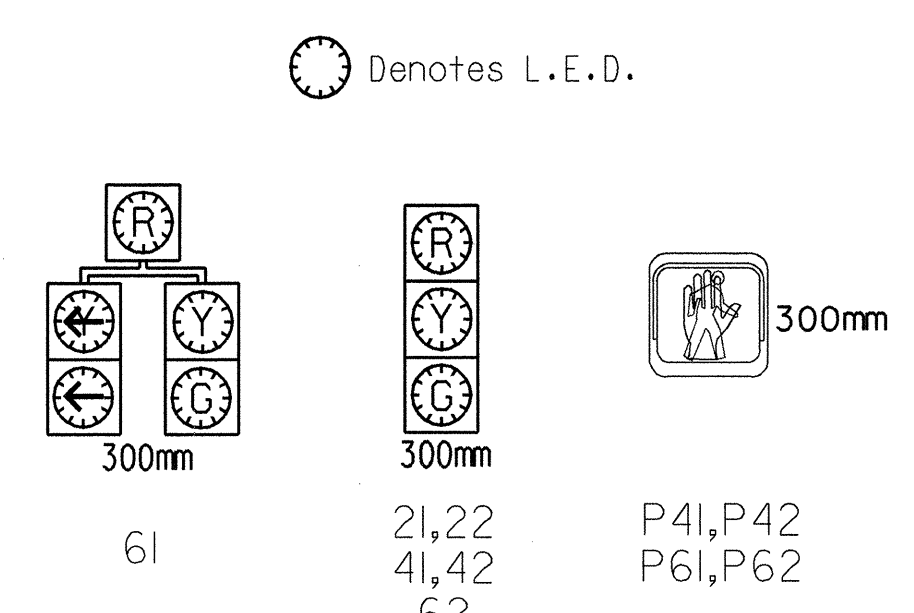


SIGNAL FACE	PHASE				
	Ø1+6	Ø2+6	Ø4+8	Ø6+8	Ø8
21,22	R	G	R	Y	
41,42	R	R	G	R	
61		G	G	R	Y
62	G	G	R	Y	
81,82	R	R	G	R	
P41,P42	DW	DW	W	DRK	
P61,P62	W	W	DW	DRK	

SIGNAL FACE I.D.

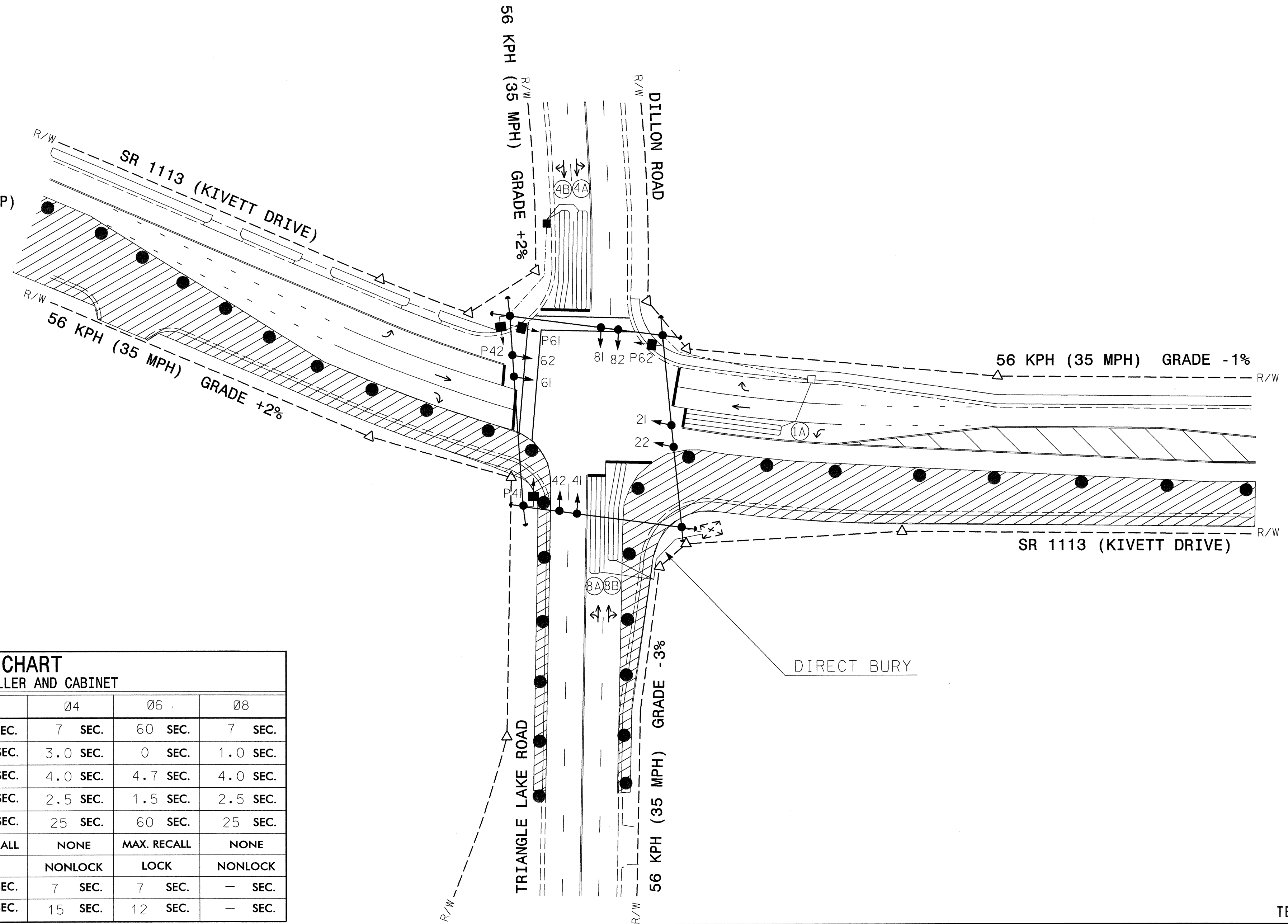
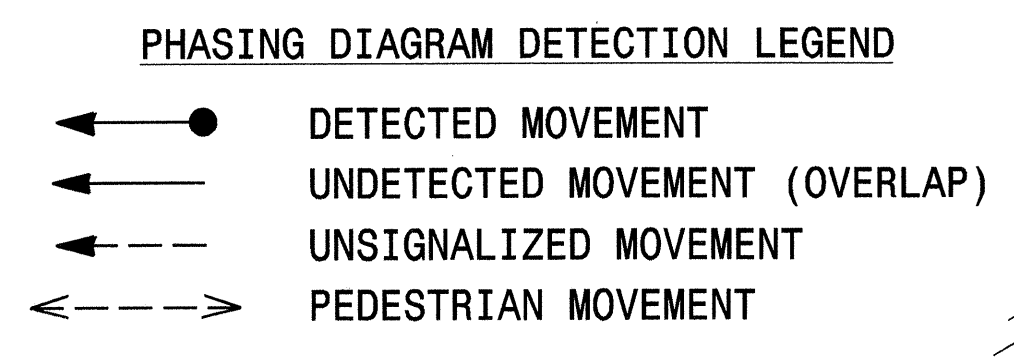


LOOP & DETECTOR UNIT INSTALLATION CHART PEEK TS-2 CONTROLLER AND CABINET											
INDUCTIVE LOOPS					DETECTOR UNITS						
LOOP NO.	SIZE (m)	TURNS	DIST. FROM STOPBAR (m)	NEW EXISTING	NEMA PHASE	NEW EXISTING	TIMING		PLACE CALL DURING PHASE	INHIBIT DELAY DURING GREEN?	
							FEATURE	TIME			
1A	1.8X18	2-4-2	0	X	Ø1	X	DELAY	15 SEC.	ALL	YES	
4A	1.8X18	2-4-2	0	X	Ø4	X	DELAY	3 SEC.	ALL	YES	
4B	1.8X18	2-4-2	0	X	Ø4	X	DELAY	10 SEC.	ALL	YES	
8A	1.8X18	2-4-2	0	X	Ø8	X	DELAY	3 SEC.	ALL	YES	
8B	1.8X18	2-4-2	0	X	Ø8	X	DELAY	10 SEC.	ALL	YES	

3 Phase Semi-Actuated
(High Point City Signal System)

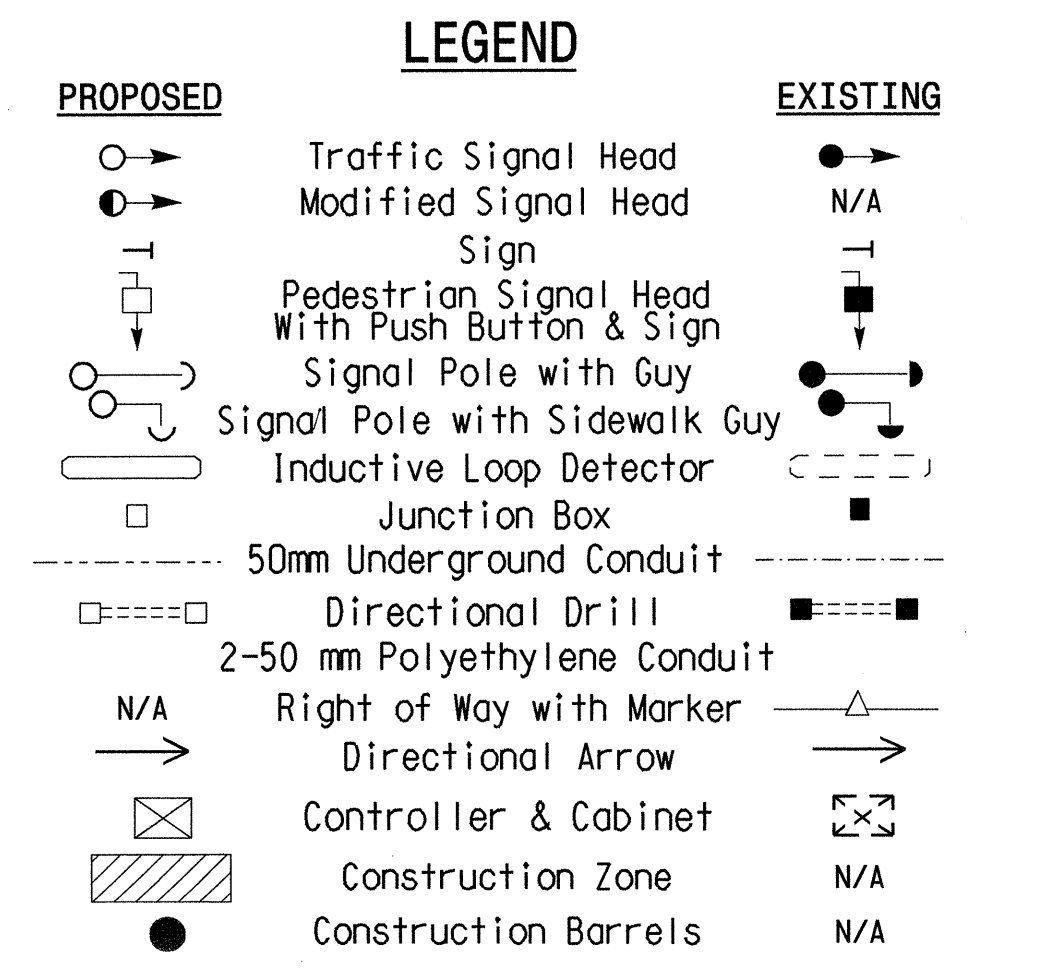
NOTES

- REFER TO "ROADWAY STANDARD DRAWINGS NCDOT" - RALEIGH, NC, DATED JANUARY 2002 AND "STANDARD SPECIFICATIONS FOR ROADS AND STRUCTURES" DATED JANUARY 2002.
- LOCATE NEW CABINET SO AS NOT TO OBSTRUCT SIGHT DISTANCE OF VEHICLES TURNING RIGHT ON RED.
- REPOSITION EXISTING SIGNAL HEADS NUMBERED 21,22, 41,42, 61,62, 81 AND 82.
- OMIT PHASE 1 DURING PHASE 2 ON.
- PROGRAM CONTROLLER TO CLEAR FROM PHASE 2+6 TO PHASE 1+6 BY PROGRESSING THROUGH PHASE 4+8 (SEE ELECTRICAL DETAILS).
- MAXIMUM TIMES SHOWN IN TIMING CHART ARE FOR FREE-RUN OPERATIONS ONLY. COORDINATED SIGNAL SYSTEM TIMING VALUES SHALL SUPERSEDE THESE VALUES.
- OMIT "WALK" AND FLASHING "DONT WALK" WITH NO PEDESTRIAN CALLS.
- DO NOT PROGRAM SIGNAL FOR FOR LATE NIGHT FLASHING OPERATION UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- PROGRAM PHASE 4 AND PHASE 8 FOR DUAL ENTRY.
- SET ALL DETECTOR UNITS TO PRESENCE MODE.
- SIGNAL SYSTEM INTERSECTION ID # 710



PLAN QUANTITIES	
Pay Item	Meters
Signal Cable	220
Messenger Cable	
Lead-in Cable	110

TIMING CHART PEEK TS-2 CONTROLLER AND CABINET					
PHASE	Ø1	Ø2	Ø4	Ø6	Ø8
MINIMUM GREEN	7 SEC.	60 SEC.	7 SEC.	60 SEC.	7 SEC.
PASSAGEGAP	1.0 SEC.	0 SEC.	3.0 SEC.	0 SEC.	1.0 SEC.
YELLOW CHANGE INT.	4.0 SEC.	4.7 SEC.	4.0 SEC.	4.7 SEC.	4.0 SEC.
RED CLEARANCE	2.0 SEC.	1.5 SEC.	2.5 SEC.	1.5 SEC.	2.5 SEC.
MAXIMUM 1	20 SEC.	60 SEC.	25 SEC.	60 SEC.	25 SEC.
RECALL POSITION	NONE	MAX. RECALL	NONE	MAX. RECALL	NONE
VEHICLE CALL MEMORY	NONLOCK	LOCK	NONLOCK	LOCK	NONLOCK
WALK	- SEC.	7 SEC.	7 SEC.	7 SEC.	- SEC.
FLASHING DON'T WALK	- SEC.	12 SEC.	15 SEC.	12 SEC.	- SEC.



TEMPORARY SIGNAL 2 - TCP PHASE 3

<p>PLANS PREPARED BY : RUMMEL KLEPPER & KAHL, LLP consulting engineers 5800 FARINGDON PLACE SUITE 105 RALEIGH, NORTH CAROLINA 27609-3960 FOR DIVISION OF HIGHWAYS</p>	<p>Prepared for the Offices of: North Carolina Department of Transportation 122 N. McDowell St., Raleigh, NC 27603</p>	<p>SR 1113 (KIVETT DRIVE) AT TRIANGLE LAKE ROAD / DILLON ROAD</p>		<p>SEAL NORTH CAROLINA PROFESSIONAL ENGINEER SEAL 19798 DONALD W. MORTON</p>					
		<p>DIVISION 07 GUILFORD COUNTY HIGH POINT</p> <p>PLAN DATE: 03-19-04 REVIEWED BY: D. WORTON</p> <p>PREPARED BY: J. COLE RK&K PROJECT NO. 302-079-SIG5</p>	<p>REVISIONS</p> <table border="1"> <tr> <th>NO.</th> <th>DATE</th> <th>INIT.</th> <th>DATE</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table>		NO.	DATE	INIT.	DATE	
NO.	DATE	INIT.	DATE						